

THE SCULLERS HEAD OF THE RIVER RACE

Organised by the Vesta Open Events Committee

Saturday 2nd December 2006 12.30pm

Instructions for Competitors – Please read carefully

General.

Numbers

You will receive two paper numbers and one rigid plastic number from the collection point nominated on your entry form – Putney (Vesta Rowing Club), Hammersmith (the ARA) or Chiswick (Mortlake AABC). The number collection points will be open from 9-30am to 11-30am on the morning of the event. Numbers may also be collected from Vesta on the evening of Friday 1st December from 6-30pm to 8-30pm.

Of the two paper numbers, one is to be fixed to the back of your vest and one to your warm-up kit by six fixing points. We supply safety pins with the numbers. Great care should be taken since if your number flaps in the wind and becomes unreadable your time **may** not be recorded.

The plastic number enclosed must be fitted into the Empacher slot on the bow of your boat.

Important Note

If the plastic number is not fitted to your boat crossing the finish line, **A TIME WILL NOT BE RECORDED** (holes can be made in the plastic numbers to obtain a more secure fixing such as bolting or putting a self-tapping screw through the Empacher slot and number).

Returning Numbers

Please return the plastic numbers to your collection point (or to Vesta Rowing Club) after the Race. Failure to return your number **will** mean that you will be invoiced for the replacement cost. Failure to settle this debt and you risk possible exclusion for you *and your club* from subsequent events organised by the Vesta Open Events Committee and you will be reported to the TRRC.

Lightweights

There will be two lightweight weighing-in stations; one at the ARA headquarters, Hammersmith and one at Vesta Rowing Club. Both stations will be open between 10.00am and 11.00am on the day of the race. Failure to weigh-in will make a sculler ineligible for a lightweight medal (they will race in the appropriate open weight category).

Max weight for Men 75.0kg

Max weight for Women 61kg

River Closure and Boating Regulations

The river will be closed by the PLA to other traffic from 11-30am at Fulham ('Putney') Rail Bridge and 11-45am at Kew Railway Bridge until 14-30pm – or until the last competitor has crossed the finish line (Notices to Mariners U17 of 2006).

Before and after the time of the river closure, Scullers should strictly observe the rules that are set out in the Tideway Rowing Code of Practice.[<http://www.thames-rrc.org/rowing-on-the-thames/navigation-on-the-river-thames>]

It is the responsibility of scullers and club officials only to enter scullers that are competent to row on the tidal Thames with respect to such aspects as steering, emergency halts and navigation, etc.

All competitors are reminded of the ARA and Thames Regional Rowing Council requirements to have ARA-compliant boat ID when on the tidal Thames.

(It should be borne in mind that the PLA can fine you up to £1000 for an absent name on a boat and that the Thames Regional Rowing Council can fine your Club up to £500)

Vesta Open Events takes their responsibility to abide by ARA Rules seriously and disqualification for any boats observed without any ID is a certainty – and we may penalise any with ID that is not fully compliant - for example not in contrasting colours and which, as a result, is not readily visible.

Vesta's website carries various links to advice for crews and scullers about rowing on the Tideway - <http://vrc.org.uk/visiting-crews.phtml>

Juniors

The ARA Competitions Committee have approved a local rule regarding younger scullers. No competitor will be allowed to compete if their date of birth is after 1 September 1990. This rule was mentioned on the entry form, which should have been signed by an appropriate authority within the sculler's club.

Marshalling

- Competitors will be marshalled from the **Surrey** (the southern) Bank between Chiswick Bridge and a point below Barnes Bridge. Competitors proceeding to the start from Putney and Hammersmith should *keep to the Surrey Bank* and will come under the control of the Marshals as they approach Barnes Bridge.
- No sculler numbered greater than 200 is to proceed above Barnes Bridge until instructed to do so by a Marshall.
- Scullers proceeding to the start must remain in single file and not overtake in the Inshore Zone adjacent to Barnes Railway Bridge Restricted Zone (Restricted Zones are new – see the Tideway Rowing Code of Practice).
- Scullers proceeding downstream must respect the rules of the Restricted Zone and also prior to the race must not turn before the "Bandstand". When they do turn they should do so by TWO ninety-degree turns – one to stop and look and check it is safe to cross the Fairway before heading directly to the bank by the SHORTEST route possible and the other *just* before reaching the bank to get in line pointing upstream. **Under no circumstances should they turn on the spot and paddle up the centre of the stream at an acute angle edging slowly to the bank.**
- Sculler No.1 will be marshalled under Chiswick Bridge, bows facing upstream, followed by No.2, No.3 etc. Odd numbers should be as close as possible to the bank; even numbers immediately outside them and scullers should keep as close together as is safe and practicable.
- **Please be in your correct marshalling position by 12-20 hrs**, when an air horn will sound.

- Scullers proceeding to the start from above Chiswick Bridge or from the Middlesex shore **MUST** be in their correct position (i.e. downstream of Chiswick Bridge) before the air horn signal. If not they must await the direction of the Marshal at the start before entering the starting line.
- After 12-20 scullers will not be allowed to cross the course or to proceed downstream through Chiswick Bridge other than by direction of the Marshals.
- At 12-25 sculler No.1 will proceed through Chiswick Bridge turning round the buoy which will be placed furthest upstream of the Surrey pier of the bridge, scullers No. 2 and 3 etc. will follow as directed by the marshalls. (There is a schematic diagram of the start area at the end of these instructions.)
- Outer garments not worn for racing must be removed before passing under Chiswick Bridge going upstream as, once turned, scullers are under Starter's Orders.
- In the event of a sculler getting into difficulties when turning and needing rescue or attention, marshals will direct following scullers to turn at the buoy placed closer to the bridge [B2 in the diagram] until the obstruction is cleared.
- The Starter will start you immediately below the bridge by saying "Number XX – GO" your starting time will be taken as you pass the University Stone downstream from the Bridge.
- **Scullers will not be allowed to join the start above Chiswick Bridge without following the correct assembly instructions.** You cannot boat upstream of Chiswick Bridge and expect to fit in at the start. You have to marshal downstream of Chiswick Bridge along with all other competitors.
- Some scullers have explicit permission from the organisers to start out of sequence and the marshalls will be informed of such changes that are known.
- **ANY SCULLER STARTING OUT OF SEQUENCE WITHOUT SUCH PERMISSION MAY BE SUBJECT TO A TIME PENALTY AT THE STARTER'S DISCRETION.**
- During the race scullers **are required to give way when being overtaken** – failure to do so may incur a time penalty or disqualification.

You and your club has a duty of care to boat only in equipment that complies with the ARA Water Safety Code in all respects, for example, your boat must carry, securely fixed to the bow, a solid ball of rubber or material of similar consistency in accordance with ARA Rules of Racing. Buoyancy compartments must be closed & watertight and heel restraints in place and effective. Random checks may be made at boating areas and any boats found unsafe will not be allowed to boat until rendered safe.

The Race

ALL SCULLERS MUST PASS ON THE OUTSIDE (MIDDLESEX) SIDE OF THE LINE OF MOORED BOATS ON THE STARBOARD (SURREY) SIDE OF THE FAIRWAY AT PUTNEY. ANY SCULLER FAILING TO COMPLY WILL BE DISQUALIFIED.

The Finish

The finish of the race at Putney is the downstream end of Putney Pier, where your time is recorded (so keep on sculling until you are past the pier – you may lose valuable seconds if you do not).

After clearing the finishing line please continue sculling through Putney Bridge well clear of other competitors before turning. There will be marshalls below the finish line – obey their instructions.

During the time of river closure (11.30 to 14.30) scullers should proceed as follows: -

- a) Scullers disembarking at Putney should turn towards the Surrey shore.
- b) Scullers proceeding up river beyond Hammersmith should keep to the Surrey bank as far as the Chiswick Steps Crossing where, under the strict control of the marshalls, they should wait in line (not bunched up) close to the bank before, only when instructed, crossing to the Middlesex bank.
- c) Scullers returning to the Hammersmith Boathouses should keep to Surrey Bank, and should wait in line (not bunched up) close to the bank just before the St. Paul's slip and not directly opposite it, before, only when instructed, crossing under the direction of the marshal at St Paul's School.

PLEASE FOLLOW THE INSTRUCTIONS OF THE MARSHALLS, STARTERS AND UMPIRES. FAILURE TO DO SO OR TO FOLLOW EITHER THE VERBAL OR WRITTEN INSTRUCTIONS TO COMPETITORS WILL RENDER YOU LIABLE TO A TIME PENALTY OR DISQUALIFICATION.

Safety & Rescue

Extensive safety and rescue facilities are employed for the race. In the event that a boat "gets into difficulty" any rescue assistance which is offered **MUST BE ACCEPTED**

1. For personal safety.
2. For safety of other competitors and to minimise the likelihood of impeding other scullers.

Random checking of boats may take place, to ensure compliance with ARA Rules including boat ID.

FAILURE TO COMPLY WITH ANY OF THESE RULES CAN RESULT IN DISQUALIFICATION

WARNING:

- All entrants in the Scullers' Head should be aware that they enter the Head at their own risk.
- They should ensure that they are fully insured against that risk.
- They should be competent scullers and competent swimmers for Tideway conditions and the advice of the ARA is that Capsize drills should have been done.
- It is their responsibility to ensure that their equipment is in good order and compliant with the ARA Rules of Racing.
- They must also be satisfied that they are sufficiently fit and competent to participate safely in the event.

Parking On The Embankment - Putney

Following the imposition by Wandsworth Borough Council of parking restrictions, competitors cannot be permitted to park on the Embankment on Race Day. Competitors can drive on to the Putney Embankment in order to unload/load boats, but cannot leave their vehicle there. Parking is available in some nearby streets or local car parks under normal restrictions and outside the control of the race organisers.

There is the possibility for a small number of boat trailers to be left on the Putney Embankment – apply to the race secretary to enquire about permission.

Results

Appeals should be made to the Chief Umpire at Ranelagh Sailing Club within one hour of the last sculler completing the course.

Results will be available on the web at <http://www.vrc.org.uk/sh/> as soon as possible after the race - and in Vesta Rowing Club on the afternoon of the race.

In the event of a dead heat between two or more, the Sculler with the earlier or earliest start number will be placed above the other(s).

Any sculler requiring an individual result sheet should leave a 9" X 4" stamped addressed envelope, in the Office at Vesta Rowing Club after the Race.

Prizes

Prizes will be presented at Vesta Rowing Club on Thursday 14th December at 19.30hrs.
Prize winners will be sent full details, via the details on their entry, after the race.

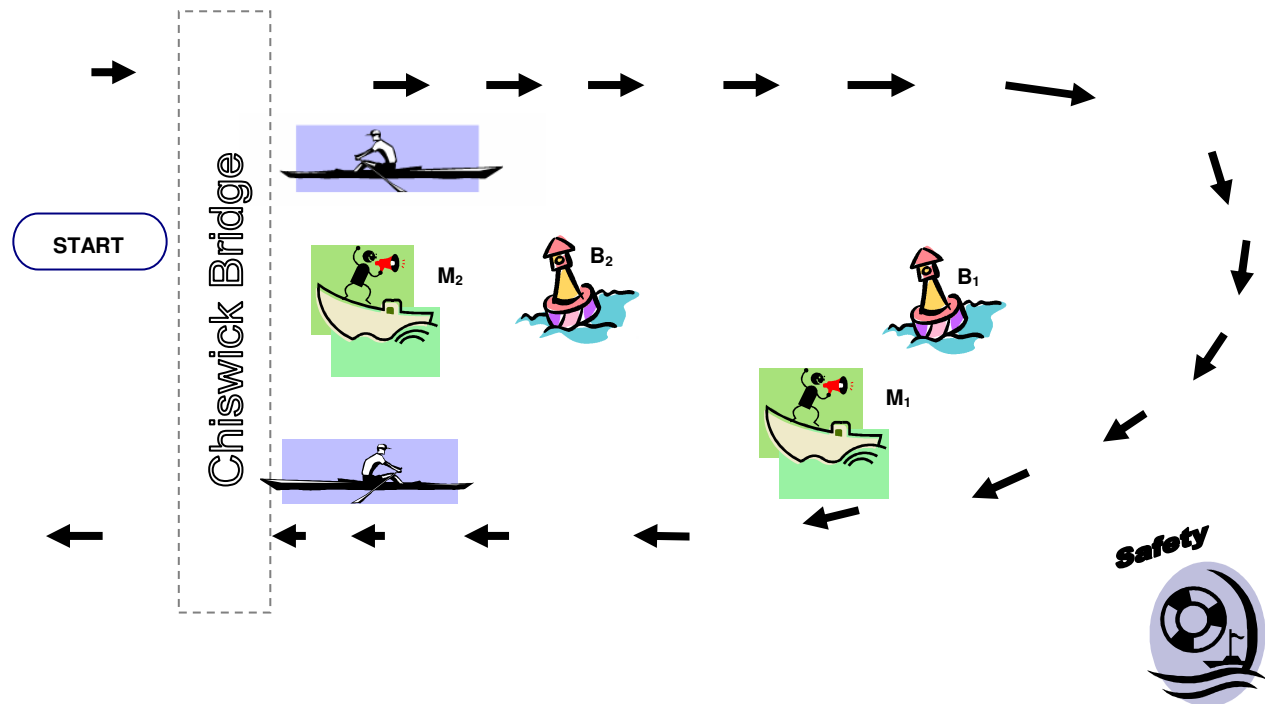
Cancellation

In the event of cancellation of the race due to inclement weather, refunds of entry fees will NOT be made. The reason for this is that we still incur the costs of running the event whether the race actually goes ahead or not.

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Vesta Open Events Committee
Vesta Rowing Club
The Embankment
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Starting Area Diagram



1. There are 2 turning buoys upstream of Chiswick Bridge - B₁ is the main turning buoy, and B₂ is the backup turning buoy.
2. There are 2 marshalls positioned upstream of Chiswick Bridge
3. M₁ is the turning marshal, positioned near the turning buoy.
4. M₂ is positioned just upstream of Chiswick Bridge to keep crews moving upstream and feeding them through to M₁.
5. There will be a Safety launch positioned upstream of B₁ patrolling the start zone
6. At the given instruction scullers are to proceed through Chiswick Bridge with warm-up gear off and ready to race.
7. Scullers are to proceed upstream of B₁ giving themselves sufficient room to effect the turn safely, moving to the centre of the river and to proceed under the marshal's instructions toward the bridge and then to the start.
8. If a sculler is in difficulty and has not managed to successfully turn around B₁ then following scullers will be directed by the M₂ to turn around B₂ whilst a rescue is in process.. When it is safe to do so, the marshalls will revert back to turning scullers around B₁.
9. Scullers must obey the all marshalling instructions, failure to do so will result in a penalty or disqualification.